



**COMMONWEALTH OF KENTUCKY
TRANSPORTATION CABINET**

Frankfort, Kentucky 40622
www.transportation.ky.gov/

Matthew G. Bevin
Governor

Greg Thomas
Secretary

June 19, 2018

CALL NO. 200
CONTRACT ID NO. 181021
ADDENDUM # 1

Subject: FAYETTE-SCOTT COUNTIES, 121GR18D021-NHPP
Letting June 22, 2018

- (1) Revised - Stationing Information Plan Sheets Pages 27-28, 33 and 39 of 185
- (2) Added - Major Pavement Repair Detail Sheet Page 48(a) of 185
- (3) Added - Blow Up/Relief Joint Repair Detail Sheet Page 48(b) of 185
- (4) Revised - Pavement Rehabilitation Summary Sheet- Page 49 of 185
- (5) Revised - Special Note - Pages 58-59 of 185
- (6) Revised - Traffic Control Plan - Pages 61-65(a) of 185
- (7) Revised - Material Summary - Pages 144-145(a) of 185
- (8) Revised - Proposal Bid Items - Pages 184-185(a) of 185

Proposal revisions are available at <http://transportation.ky.gov/Construction-Procurement/>.

If you have any questions, please contact us at 502-564-3500.

Sincerely,

A handwritten signature in black ink that reads "Rachel Mills".

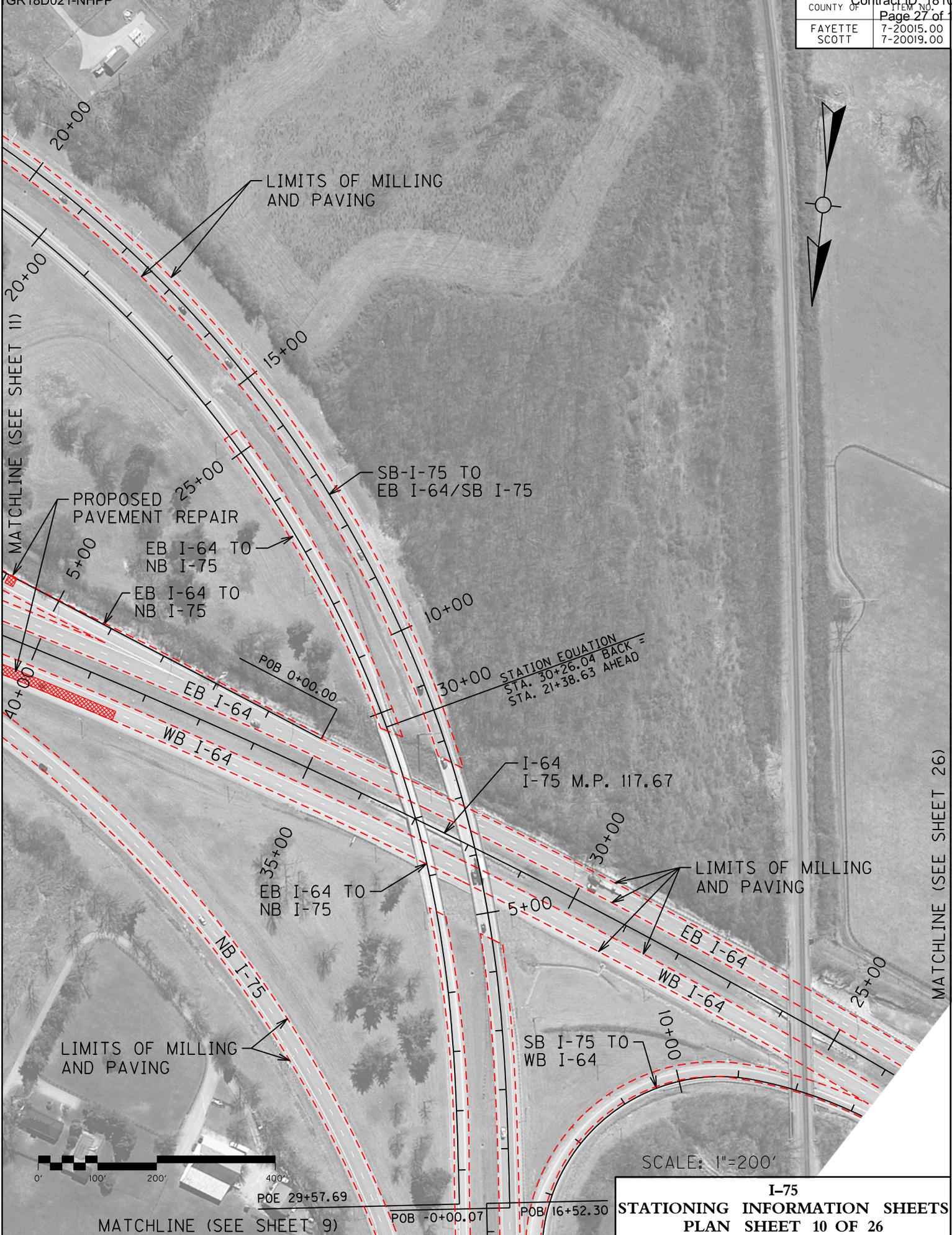
Rachel Mills, P.E.
Director
Division of Construction Procurement

RM:mr
Enclosures



An Equal Opportunity Employer M/F/D

| | |
|-----------|------------|
| COUNTY OF | ITEM NO. |
| FAYETTE | 7-20015.00 |
| SCOTT | 7-20019.00 |



STATION EQUATION
 STA. 30+26.04 BACK =
 STA. 21+38.63 AHEAD

I-64
 I-75 M.P. 117.67

SCALE: 1"=200'

I-75
STATIONING INFORMATION SHEETS
PLAN SHEET 10 OF 26

POE 29+57.69

POB -0+00.07

POB 16+52.30

POB 0+00.00

MATCHLINE (SEE SHEET 9)

MATCHLINE (SEE SHEET 26)



| | |
|-----------|------------|
| COUNTY OF | ITEM NO. |
| FAYETTE | 7-20015.00 |
| SCOTT | 7-20019.00 |

FAYETTE - SCOTT COUNTIES
121GR18D021-NHPP

MATCHLINE (SEE SHEET 12)

60+00

SB I-75
TO EB I-64/SB I-75

35+00

EB I-64/SB I-75
WB I-64/NB I-75

55+00

U.S. 25 30+00

LIMITS OF MILLING
AND PAVING

LIMITS OF MILLING
AND PAVING

50+00

OVERHEAD SIGN

U.S. 25
I-75 M.P. 117.45

DO NOT DISTURB
EXISTING HIGH
FRICTION SURFACE

25+00

EB I-64 TO
NB I-75

15+00

10+00

MATCHLINE (SEE SHEET 10)

45+00

NB I-64
NB I-75

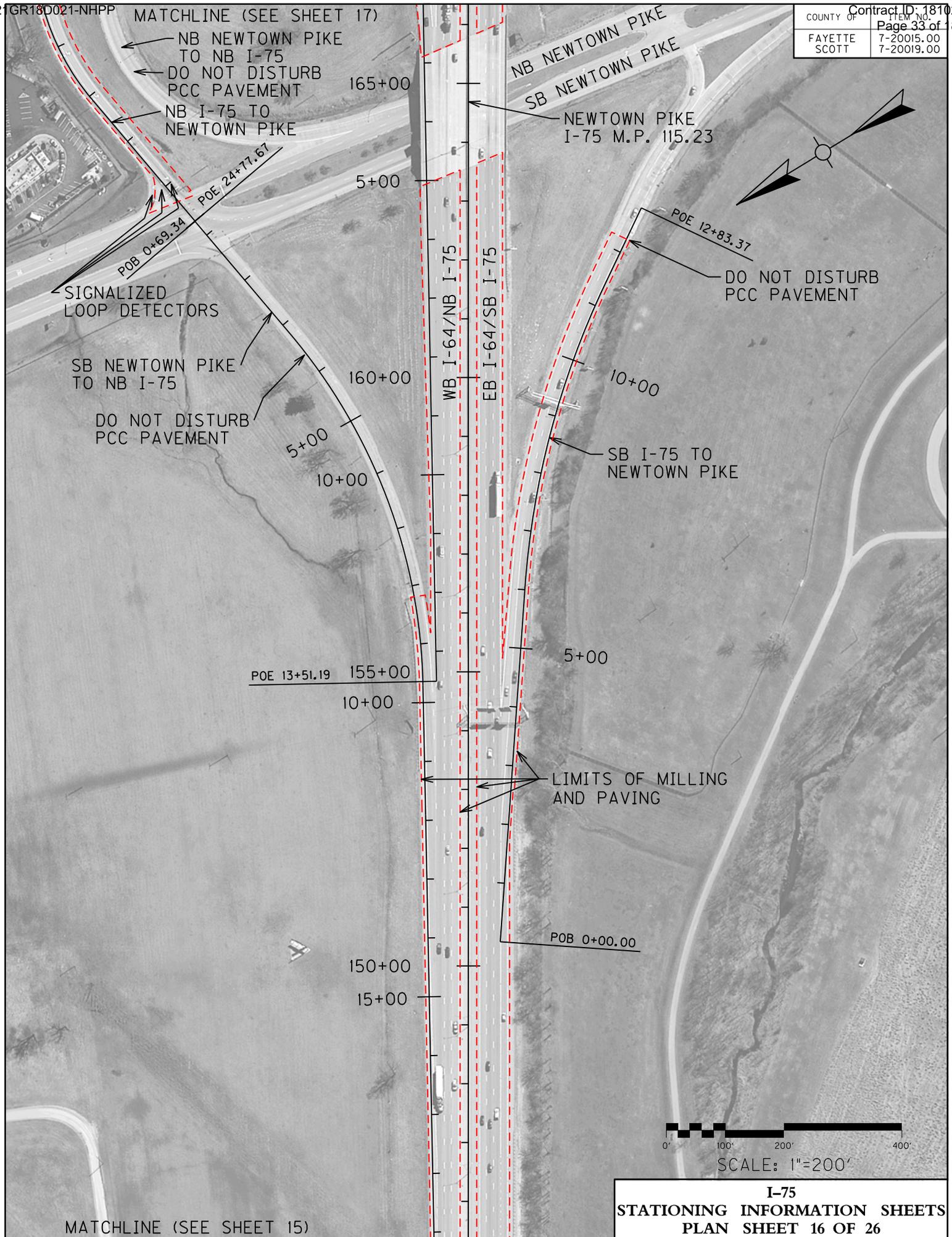
PROPOSED PAVEMENT
REPAIR



SCALE: 1"=200'

I-75
STATIONING INFORMATION SHEETS
PLAN SHEET 11 OF 26

| | |
|-----------|------------|
| COUNTY OF | ITEM NO. |
| FAYETTE | 7-20015.00 |
| SCOTT | 7-20019.00 |



SCALE: 1"=200'

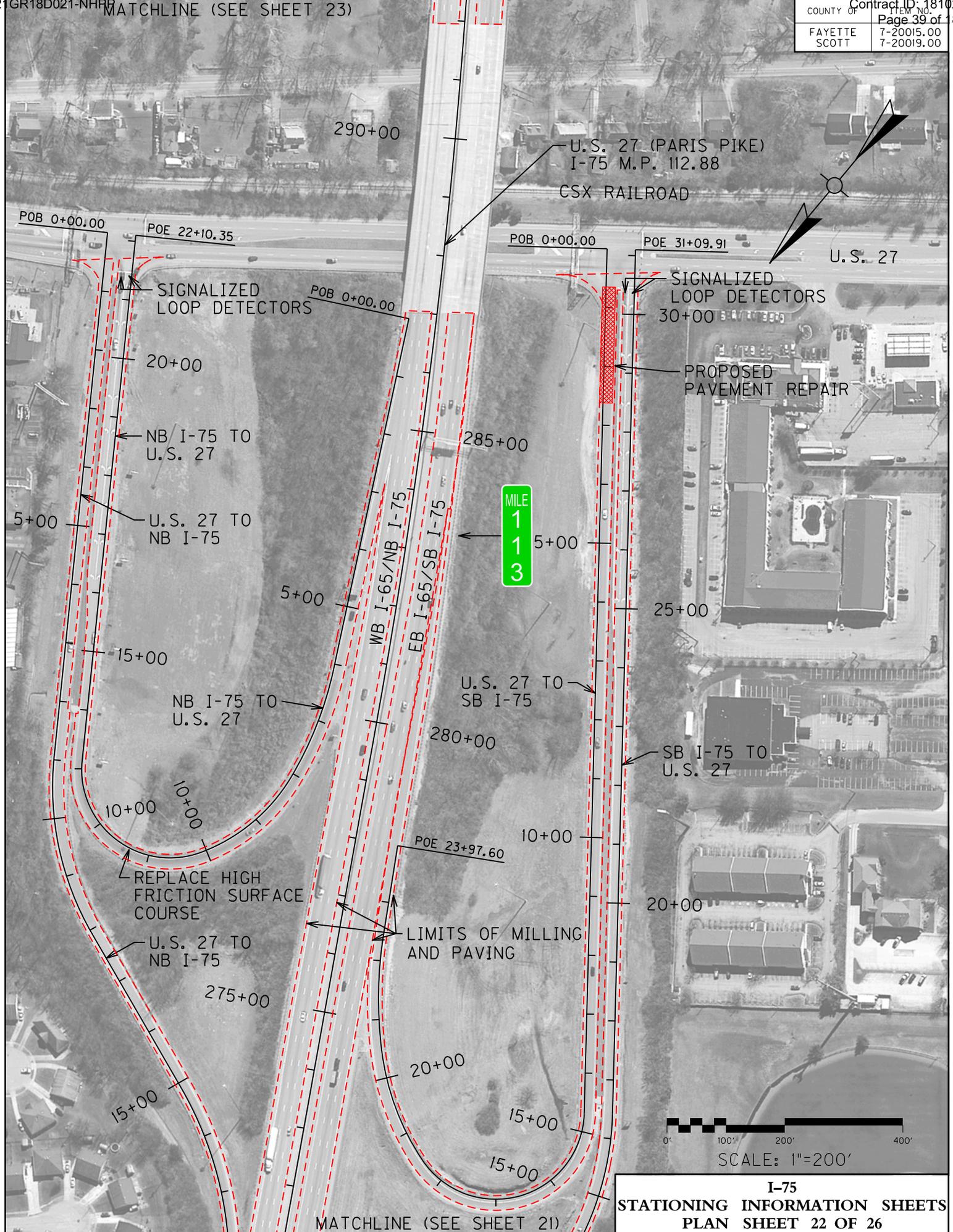
I-75
STATIONING INFORMATION SHEETS
PLAN SHEET 16 OF 26

MATCHLINE (SEE SHEET 15)

MATCHLINE (SEE SHEET 17)

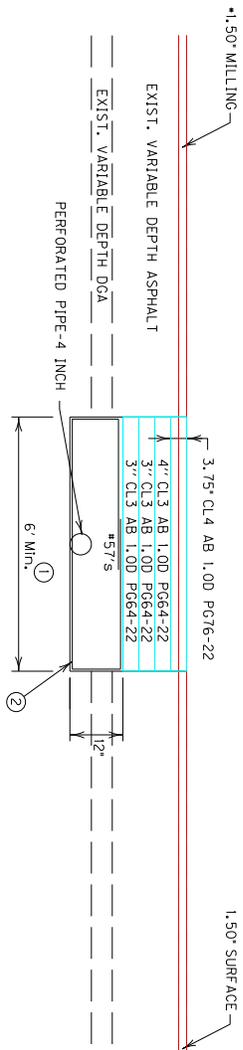
MATCHLINE (SEE SHEET 23)

| | |
|-----------|------------|
| COUNTY OF | ITEM NO. |
| FAYETTE | 7-20015.00 |
| SCOTT | 7-20019.00 |



I-75
STATIONING INFORMATION SHEETS
PLAN SHEET 22 OF 26

| | |
|------------------|----------------------|
| COUNTY OF | ITEM NO. |
| FAYETTE SCOTT | 7-20015 & 7-20019 |



MAJOR PAVEMENT REPAIR DETAIL
FAYETTE & COUNTY

① APPROXIMATE LOCATIONS ARE NOTED IN THE PROPOSAL. SPECIFIC LOCATIONS WILL BE DETERMINED BY THE ENGINEER IN THE FIELD. BEFORE REMOVAL OF THE EXISTING MATERIAL, SAW CUTS SHALL BE MADE ON EACH SIDE OF THE AREA TO BE REMOVED. THIS OPERATION SHALL BE PERFORMED ON THE OUTSIDE SHOULDER AND DRIVING LANES WHERE DIRECTED.

AFTER ALL EXISTING MATERIAL HAS BEEN REMOVED THEN EACH COURSE OF BACKFILL MATERIAL SHALL BE COMPACTED TO THE PROPER DENSITY FOR THE MATERIAL BEING PLACED AS REQUIRED IN THE STANDARD SPECIFICATIONS. THE 4" PERFORATED PIPE SHALL NOT BE WRAPPED AND ONLY COARSE AGGREGATE SHALL BE USED.

THE CONTRACT UNIT BID PRICE PER SQ YD FOR PAVEMENT REPAIR (MAJOR) SHALL INCLUDE ROADWAY EXCAVATION, REMOVING PAVEMENT, SAW CUTTING, PERFORATED PIPE-4 INCH, CRUSHED AGGREGATE SIZE NO. 57, ASPHALT BASE, FABRIC-GEOTEXTILE TYPE IV AND ALL INCIDENTALS NECESSARY TO COMPLETE THE INSTALLATION AS DETAILED.

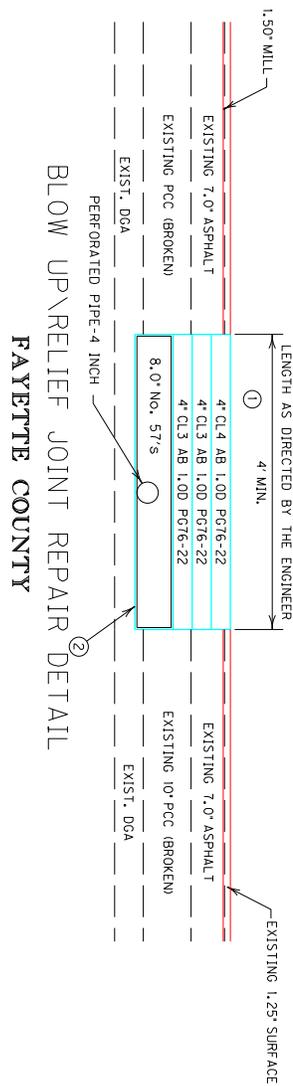
PLACE THE PERFORATED PIPE AT THE LOW POINT OF THE DIGOUT.

THE NON-PERFORATED OUTLET PIPE AND HEADWALL WILL BE PAID FOR SEPARATELY. ANY EXCAVATION OR BACKFILL REQUIRED WILL BE INCIDENTAL TO THE LENGTH OF NON-PERFORATED PIPE.

TRAFFIC SHALL BE ALLOWED TO TRAVEL ON REPAIRS FOR A MINIMUM OF 7 DAYS PRIOR TO MILLING AND RESURFACING.

② FABRIC-GEOTEXTILE TYPE IV REQUIRED AROUND COARSE AGGREGATE. OVERLAP FABRIC ONE FOOT.

| | |
|------------------|----------------------|
| COUNTY OF | ITEM NO. |
| FAYETTE SCOTT | 7-20015 & 7-20019 |



① APPROXIMATE LOCATIONS ARE NOTED IN THE PROPOSAL. SPECIFIC LOCATIONS WILL BE DETERMINED BY THE ENGINEER IN THE FIELD. BEFORE REMOVAL OF THE EXISTING MATERIAL, SAW CUTS SHALL BE MADE ON EACH SIDE OF THE AREA TO BE REMOVED. THIS OPERATION SHALL BE PERFORMED ON THE SHOULDER AND DRIVING LANES WHERE DIRECTED.

AFTER ALL EXISTING MATERIAL HAS BEEN REMOVED THEN EACH COURSE OF BACKFILL MATERIAL SHALL BE COMPACTED TO THE PROPER DENSITY FOR THE MATERIAL BEING PLACED AS REQUIRED IN THE STANDARD SPECIFICATIONS.

TRAFFIC SHALL BE ALLOWED TO TRAVEL ON REPAIRS FOR A MINIMUM OF 7 DAYS PRIOR TO MILLING AND RESURFACING.

THE CONTRACT UNIT BID PRICE PER SQ YD FOR "BLOW UP\RELIEF JOINT REPAIR DETAIL" SHALL INCLUDE SAW CUTTING THROUGH THE ASPHALT, REMOVING PAVEMENT, ROADWAY EXCAVATION, PERFORATED PIPE-4 INCH, CRUSHED AGGREGATE SIZE NO. 57, ASPHALT BASE, FABRIC-GEOTEXTILE TYPE IV, AND ALL INCIDENTALS NECESSARY TO COMPLETE THE INSTALLATION AS DETAILED.

4 INCH PERFORATED PIPE WILL BE PLACED AT THE LOW POINT OF THE TRENCH AND TIED TO THE EXISTING EDGE DRAIN SYSTEM.

ONCE WORK HAS BEGUN THE CONTRACTOR WILL WORK CONTINUOUSLY UNTIL THE REPAIR IS COMPLETED

② FABRIC-GEOTEXTILE TYPE IV REQUIRED AROUND COARSE AGGREGATE. OVERLAP FABRIC ONE FOOT.

NOT TO SCALE

I-75\I-64
DETAIL SHEET

PAVEMENT REHABILITATION SUMMARY
I-75 FAYETTE COUTY MILEPOST 111.95 TO 120.79
I-75 SCOTT COUTY MILEPOST 120.79 TO 121.12
ITEM NUMBER 7-20015.00 & 7-20019.00

| ITEM NUMBER | ITEM | NOTE | FAYETTE COUNTY | SCOTT COUNTY | PROEJCT TOTALS | UNIT |
|-------------|---|------|----------------|--------------|----------------|------|
| 1 | DGA BASE | (5) | 14 | | 14 | TON |
| 193 | ASPHALT SCRATCH COURSE PG76-22 | (4) | 1,972 | 239 | 2,211 | TON |
| 342 | CL4 ASPH SURF 0.38A PG76-22 | (4) | 45,524 | 1,467 | 46,991 | TON |
| 356 | ASPHALT MATERIAL FOR TACK | (4) | 233 | 7 | 240 | TON |
| 1020 | PERF PIPE HEADWALL TY 1-4 IN | | 1 | | 1 | EACH |
| 2367 | GUARDRAIL END TREATMENT TYPE 1 | | 1 | | 1 | EACH |
| 2381 | REMOVE GUARDRAIL | | 175 | | 175 | LF |
| 2562 | TEMPORARY SIGNS | | 2,000 | 500 | 2,500 | SQFT |
| 2568 | MOBILIZATION | | 1 | 1 | 2 | LS |
| 2569 | DEMobilIZATION | | 1 | 1 | 2 | LS |
| 2650 | MAINTAIN AND CONTROL TRAFFIC | | 1 | 1 | 2 | LS |
| 2671 | PORTABLE CHANGEABLE MESSAGE SIGN | | 10 | 2 | 12 | EACH |
| 2676 | MOBILIZATION FOR MILL & TEXT | | 1 | 1 | 2 | LS |
| 2677 | ASPHALT PAVE MILLING & TEXTURING | (4) | 41,840 | 1,706 | 43,546 | TON |
| 2696 | SHOULDER RUMBLE STRIPS | | 248,188 | 7,121 | 255,309 | LF |
| 2775 | ARROW PANEL | | 4 | | 4 | EACH |
| 4792 | CONDUIT (1") | | 110 | | 110 | LF |
| 4795 | PVS CONDUIT (2") SCH 80 | | 420 | | 420 | LF |
| 4811 | JUNCTION BOXES TYPE B | | 7 | | 7 | EACH |
| 4820 | TRENCHING & BACKFILLING | | 615 | | 615 | LF |
| 4830 | LOOP WIRE | | 3,830 | | 3,830 | LF |
| 4850 | CABLE No. 14/1 PAIR | | 4,295 | | 4,295 | LF |
| 4895 | LOOP SAW SLOT AND FILL | | 1,545 | | 1,545 | LF |
| 6401 | FLEXIBLE DELINEATOR POST-M/W | | 533 | 9 | 542 | EACH |
| 6404 | FLEXIBLE DELINEATOR POST-M/Y | | 233 | | 233 | EACH |
| 6405 | SBM ALUM SHEET PANEL SIGNS | (6) | 111 | | 111 | SQFT |
| 6412 | STEEL POST MILE MARKERS | | 14 | 2 | 16 | EACH |
| 6511 | PAVE STRIPING-TEMP PAINT -6 IN | | 602,671 | 17,560 | 620,231 | LF |
| 6556 | PAVE STRIPING-DUR TY 1-6 IN W | | 3,764 | | 3,764 | LF |
| 6557 | PAVE STRIPING-DUR TY 1-6 IN Y | | 2,675 | | 2,675 | LF |
| 6568 | PAVE MARKING-THERMO STOP BAR-24 IN | | 90 | | 90 | LF |
| 06574 | PAVE MARKING - THERMO CURVE ARROW | | 41 | | 41 | EACH |
| 6575 | PAVE MARKING - THERMO COMBO ARROW | | 5 | | 5 | EACH |
| 10020NS | FUEL ADJUSTMENT | | 73,931 | 2,654 | 76,585 | DOLL |
| 10030NS | ASPHALT ADJUSTMENT | | 185,694 | 6,666 | 192,360 | DOLL |
| 20071EC | JOINT ADHESIVE | | 447,843 | 14,031 | 461,874 | LF |
| 20100ES842 | PAVE MARK TEMP PAINT LINE ARROW | | 46 | | 46 | EACH |
| 20411ED | LAW ENFORCEMENT OFFICER | | 1,000 | 500 | 1,500 | hour |
| 20509ED | BLOW UP/RELIEF JOINT | | 75 | | 75 | SQYD |
| 20757ED | PAVEMENT REPAIR (MINOR) | | 785 | | 785 | SQYD |
| 20757ED | PAVEMENT REPAIR (MAJOR) | | 200 | | 200 | SQYD |
| 21802EN | GUARDRAIL-STEEL W BEAM-S FACE (7 FT POST) | | 225 | | 225 | LF |
| 22692NS714 | PAVEMENT MARKING - THERMO LETTERS | | 12 | | 12 | EACH |
| 23603EC | REMOVE/REPLACE COPY ON PANEL SIGNS | (6) | 76 | | 76 | SQFT |
| 23229EC | HIGH FRICTION SURFACE TREATMENT | | 1,250 | | 1,250 | SQYD |
| 24189ER | DURABLE WATERBORNE MARKING-6 IN W | | 166,160 | 5,281 | 171,441 | LF |
| 24190ER | DURABLE WATERBORNE MARKING-6 IN Y | | 118,134 | 3,499 | 121,633 | LF |
| 24191ER | DURABLE WATERBORNE MARKING-12 IN W | (1) | 17,041 | | 17,041 | LF |
| 24489EC | INLAID PAVEMENT MARKER | | 3,475 | 88 | 3,563 | EACH |
| 24679ED | PAVE MARKING - THERMO CHEVRON | | 10,719 | | 10,719 | SQFT |
| 24781EC | INTELLIGENT COMPACTION FOR ASPHALT | (2) | 45,524 | 1,467 | 46,991 | TON |
| 24891EC | PAVE MOUNT INFRARED TEMP EQUIP | (2) | 4,966,237 | 156,664 | 5,122,901 | SQFT |
| 24899EC | PAVE MARKING - THERMO ELONG ROUTE SHIELD | (3) | 11 | | 11 | EACH |
| 24900EC | PVC CONDUIT (1 1/4") SCH 80 | | 560 | | 560 | LF |
| 24963ED | LOOP TEST | | 21 | | 21 | EACH |

NOTE: Quantities from all roadway summary sheets have been carried over and included in this General Summary

- 1 Quantity included for arrows at lane drop approaching Newtown Pike exit and arrows used with thermo along route shields.
- 2 Includes mainline and ramp quantities for driving lanes and shoulders.
- 3 To be placed at NB MPs 117.12 & 116.80 and SB MPs 118.50 & 118.75.
- 4 Includes quantity for milling and paving of full inside shoulder from sta. 190+40 to sta. 191+88
- 5 For use on guardrail end treatment ty1 installation.
- 6 Sign dimensions should be verified in the field.

representative must all sign off on this sheet before payment may be made.

5. All “green” milepost signs shall be replaced with this project. Payment for these signs will be made by “each” for the bid item “Steel Post Mile Markers”. Any damage will be replaced at the contractor’s expense.
6. Any roadway signs that are damaged during construction are to be replaced at the contractor's expense.
7. Any light poles that are damaged during construction are to be replaced at the contractor's expense.
8. The existing edge drain system is to be preserved. Care should be taken when the asphalt is removed and replaced, any edge drains damaged during these activities will be replaced at the contractor’s expense.
9. Delineators shall meet the requirements of Section 830 and 838 of the Standard Specifications. Delineators shall be placed in accordance with Section 3F of the M.U.T.C.D.
10. Quantities have been included in the General Summary for minor pavement repair. The three locations are as follows:
 - Paris Pike Ramp C Sta. 0+65 to 2+62 (482 sq. yds)
 - Iron Works Pike Ramp 16+34 to 18+42 (278 sq. yds)
 - EB I-64 to NB I-75 Ramp C Sta. 5+80 to 5+95 (25 sq. yds)

The Engineer will determine the ultimate locations that will be repaired based upon the condition of the pavement at the time the repairs are accomplished. The repair locations listed may be lengthened, shortened, or eliminated completely if the conditions are such that modification of the locations would be deemed desirable by the Department.

11. Quantities have been included in the General Summary for major pavement repair. There are two approximately 75 ft locations within the following MP location:
 - WB I-64 MP 74.58 to WB I-64 MP 74.65 (**Right lane only**)

The Engineer will determine the ultimate locations that will be repaired based upon the condition of the pavement at the time the repairs are accomplished. The repair locations listed may be lengthened, shortened, or eliminated completely if the conditions are such that modification of the locations would be deemed desirable by the Department.

Within this same above MP range (**Right lane only**) there are approximately 12 blow up relief joint repairs. The Engineer will determine the ultimate locations that will be repaired based upon the condition of the pavement at the time the repairs are accomplished. The blowup relief joints are approximately 14 feet wide (12 ft travel lane plus 1 ft on either side). See Base Failure Detail for trench width. Quantities have been included in the General Summary for the blow up relief joint repairs.

12. Pavement rideability requirements in accordance with section 410 category A of the standard specifications shall apply on this project.
13. The Department will accept the compaction of asphalt mixtures furnished for the driving lanes at one inch or greater on this project by Option A according to subsections 402 and 403 of the Standard Specifications, Current Edition. The Department will accept the compaction of all other mixtures by option B.
14. The Contractor shall be responsible for the repair of any asphalt or concrete in the travelled lanes that becomes detrimental or hazardous to the travelling public during construction. Areas needing repair will be at the discretion of the Engineer.
15. Remove the existing guardrail end treatment type 3 at SB MP 118.16 and replace with end treatment type 1. Remove 175 linear feet of existing guardrail at the end treatment and construct 225 linear feet of new guardrail along with the new End Treatment Type 1. DGA shall be used for grading for the new End Treatment Type 1. DGA will be measured and paid at the contract unit price per "ton".
16. Replace perforated pipe headwall and junction box lid at SB MP 114.01. Regrade slope to drain. All materials required for installing perf pipe headwall, replacing lid and regrading slope shall be incidental to bid item PERF PIPE HEADWALL TY 1-4 IN.
17. The inside shoulders in both directions are to be milled and resurfaced from approximate station 190+40 to 191+88. The new surface is to be graded to drain to the existing median barrier box inlet at MP 121.10. The work will be measured and paid at the contract bid prices for "Asphalt Pavement Milling and Texturing" and "CL4 ASPH SURF 0.38A PG76-22".
18. There are three panel signs to be modified with this project. Details of these signs are included in this proposal. Two of the signs include removing panels and replacing with new panels showing "EXIT ONLY". This work will be measured and paid at the contract unit bid price per square foot for "SBM ALUMINUM PANEL SIGNS". The third sign involves changing the direction arrows on the existing sign from straight to slanted to the right. This work will be measured and paid at the contract unit bid price per square foot for "REMOVE/REPLACE COPY ON PANEL SIGNS".
19. Striping modifications at exit and entrance ramps are also included with this project. Striping details are included in this proposal. Striping shall be as directed/approved by the Engineer.

Traffic Control Plan
Fayette County and Scott County
I-75
Page 2 of 7

PROJECT PHASING & CONSTRUCTION PROCEDURES

The contractor **shall maintain a minimum of one lane** in each direction during the hours of 9 pm to 6 am all days. All lanes must be striped and open at all other times and hours unless otherwise stated in this proposal or approved by the Engineer. No lane closures will be permitted the following days:

August 31, 2018 – September 3, 2018 Labor Day Weekend

The contractor **shall maintain a minimum of one lane** in the Westbound direction of I-64 for the pavement repair from MP 74.58 to MP 74.65 for **one weekend** from the hours of 8 pm Friday to 6 am Monday. Weekend must be approved by the Engineer. Once the pavement has been removed, the Contractor must work continuously until the pavement has been replaced back to existing grade. Traffic shall be allowed to travel on repairs for a minimum of 7 days prior to milling and resurfacing. All lanes must be striped and open at all other times and hours unless otherwise stated in this proposal or approved by the Engineer. No lane closures will be permitted the following days:

August 31, 2018 – September 3, 2018 Labor Day Weekend

Lane closure times may also be restricted during home football games for the University of Kentucky. The minimum clear lane width will be 12 feet. Use a lane closure all times when work is performed in the lane or adjacent shoulder. Shoulders are NOT to be used as temporary travel lanes unless otherwise directed or approved by the Engineer. If shoulders are utilized, perform any maintenance of the shoulder as deemed necessary by the Engineer to maintain traffic. All removal of existing striping shall be by water blasting, unless otherwise directed by the Engineer, and this work shall be considered incidental to “Maintain and Control Traffic.” Remove edge lines as necessary and approved by the Engineer throughout the project. Paint temporary edge lines through the lane closure. All edge lines must be painted to the existing configuration and open to traffic by 6 am each morning.

All pavement edge transitions must be smooth and level before opening lanes up to traffic. A lane closure must be in place during all times that pavement edge drop-offs are present (see Pavement Edge Drop-off note).

The Engineer will determine exact locations of pavement repairs, at the time of construction. Once removal of pavement at a repair location has begun, work continuously within the parameters outlined above to complete the work and eliminate the “hole”. Place Type III Barricades immediately in front of pavement repair areas. Once pavement removal at a site has begun, pavement replacement must be completed within the time a lane closure is allowed.

Access to all ramps at all interchanges on the project shall be maintained always unless otherwise stated in this proposal or directed by the Engineer. Single lane ramps may be closed for short periods of time for milling and paving. All ramp closures must be approved by the Engineer a minimum of one week prior to the scheduled closure. All diversions to access ramps in areas of lane closures shall be approved by the Engineer prior to implementing the lane closure.

Note that lane shifts are required throughout the project. See the Exhibits for lane locations and widths. Stripe according to the MUTCD.

During the days and hours when a lane closure is allowed, implement the following procedures: Maintain traffic as specified in the phasing notes. Any other work not requiring traffic lane widths to be restricted due to barrels or equipment encroaching into the interior lanes can be done during the remaining hours when all lanes of traffic must be maintained. Please refer to the “Special Note for Fixed Completion Date and Liquidated Damages” for damage rates per hour associated with failure to maintain the required number of lanes during the specified time. Once pavement milling at a site has begun, pavement must be completed within the time a lane closure is allowed. Liquidated Damages, at the rate specified per hour in the “Special Note for Fixed Completion Date and Liquidated Damages”, will be assessed for each hour the existing number of lanes is not maintained.

The contractor must notify the Engineer at least fourteen (14) days prior to beginning construction in either direction.

PHASE I – 3 AND 4 LANE SECTIONS

Shift traffic to the outside lanes and close the inside lane and shoulder to traffic. Mill existing pavement and resurface on the inside lanes and 4 feet of the inside shoulders as shown or directed by the Engineer. Once the pavement has been removed, the Contractor must work continuously until the pavement has been replaced back to existing grade.

PHASE II – 3 AND 4 LANE SECTIONS

Maintaining traffic on the outside lanes (lane 3 and 4), close lane 2 and the inside lane to traffic. Mill existing pavement and resurface on lane 2 as shown or directed by the Engineer. Once the pavement has been removed, the Contractor must work continuously until the pavement has been replaced back to existing grade.

PHASE III – 3 AND 4 LANE SECTIONS

Shift traffic to the inside lanes (lanes 1 and 2). Close the outside lane and the outside shoulder to traffic on the three lane sections and lane 3 on the four-lane section. Mill existing pavement and resurface on lane 3 and 4 feet of the outside shoulders, in the three lane sections and just lane 3 in the four lane sections as shown or directed by the Engineer. Ramp milling and paving in the 3 lane sections shall also be completed during this phase as approved or directed by the Engineer. Once the pavement has been removed, the Contractor must work continuously until the pavement has been replaced back to existing grade. Access to all entrance and exit ramps is to be maintained at all times unless otherwise directed by the Engineer.

PHASE IV – 4 LANE SECTIONS

Shift traffic to the inside three lanes and close the outside lane and the outside shoulder to traffic in the four lane sections only unless otherwise directed by the Engineer. Mill existing pavement and resurface on the outside lane (lane 4) and 4 feet of the outside shoulders as shown or directed by the Engineer. Ramp milling and paving in the 4 lane sections shall also be completed during this phase as approved or directed by the Engineer. Once the pavement has been removed, the Contractor must work continuously until the pavement has been replaced back to existing grade. Access to all entrance and exit ramps is to be maintained at all times unless otherwise directed by the Engineer.

Traffic Control Plan
Fayette County and Scott County
I-75
Page 4 of 7

PHASE V – 3 AND 4 LANE SECTIONS

Place permanent striping and markers throughout the project utilizing temporary lane closures like the above described closures used for milling and paving. Access to all entrance and exit ramps is to be maintained at all times unless otherwise directed by the Engineer.

LANE CLOSURES

Limit the lengths of lane closures to only that needed for actual operations in accordance with the phasing specified herein, or as directed by the Engineer. Limit lane closures to allow a minimum of one lane open per direction at any given time. Contrary to section 112, lane closures will **NOT** be measured for payment, but are considered incidental to Maintain and Control Traffic.

SIGNS

Additional traffic control signs in addition to normal lane closure signing detailed on the Standard Drawings may be required by the Engineer. Additional signs needed for lane closures may include, but are not limited to, dual mounted TRUCKS USE LEFT/RIGHT LANE, LEFT/RIGHT LANE CLOSED 1 MILE, LEFT/RIGHT LANE CLOSED 2 MILES, LEFT/RIGHT LANE CLOSED 3 MILES, SLOWED/STOPPED TRAFFIC AHEAD. Signage for reduced speed limits and double fine work zones will be furnished, relocated, and maintained by the Contractor.

Contrary to section 112, individual signs will be measured only once for payment, regardless of how many times they are set, reset, removed, and relocated during the duration of the project. Replacements for damaged signs or signs directed to be replaced by the Engineer due to poor legibility or reflectivity will not be measured for payment.

A quantity of signs has been included for lane shifts, "Roadwork Ahead" signs on entrance ramps, and extra Double Fine signs and Speed Limit signs between interchanges to be paid only once no matter how many times they are moved or relocated.

FLASHING ARROWS

Flashing arrows will be paid for once, no matter how many times they are moved or relocated. The Department **WILL NOT** take possession of the flashing arrows upon completion of the work.

PORTABLE CHANGEABLE MESSAGE SIGNS

Provide portable changeable message signs in advance of and within the project at locations to be determined by the Engineer. If work is in progress concurrently in both directions, or if more than one lane closure is in place in the same direction of travel, provide additional portable changeable message signs. Place portable changeable message signs one mile in advance of the anticipated queue at each lane closure. As the actual queue lengthens and/or shortens relocate or provide additional portable changeable message signs so that traffic has warning of slowed or stopped traffic at least one mile but not more than two miles before reaching the end of the actual queue. The locations designated may vary as the work progresses. The messages required to be provided will be designated by the Engineer. The portable changeable message signs will be in operation always. In the event of damage or mechanical/electrical failure, the Contractor will repair or replace the

Traffic Control Plan
Fayette County and Scott County
I-75
Page 5 of 7

Portable Changeable Message Sign immediately. Portable Changeable Message Signs will be paid for once, no matter how many times they are moved or relocated. The Department **WILL NOT** take possession of the signs upon completion of the work.

BARRELS

Barrels are to be used for channelization or delineation and will be incidental to "MAINTAIN AND CONTROL TRAFFIC" according to Section 112.04.01. Replacements for damaged barrels directed by the Engineer to be replaced due to poor condition or reflectivity will not be measured for payment.

TRUCK MOUNTED ATTENUATORS

Furnish and install MUTCD approved Truck Mounted Attenuators in advance of work areas when workers are present less than 12 feet from traffic. If there is less than 500 feet between work sites, only a single TMA will be required at a location directed by the Engineer. Locate the TMAs at the individual work sites and move them as the work zone moves within the project limits. All details of the TMA installations shall be approved by the Engineer. Truck Mounted Attenuators will not be measured for payment but are incidental to Maintain and Control Traffic. The Department **WILL NOT** take possession of the TMAs upon completion of the work.

PAVEMENT MARKINGS

If lane closures are in place during nighttime hours, remove or cover the lenses of raised pavement markers that do not conform to the traffic control scheme in use, or as directed by the Engineer. Replace or uncover lenses before a closed lane is reopened to traffic. No direct payment will be made for removing and replacing or covering and uncovering the lenses but will be incidental to "Maintain and Control Traffic".

Place temporary and permanent striping in accordance with Section 112 and Section 71, except that:

1. Temporary and permanent striping will be 6" in width; and
2. If the contractor's operations or phasing requires temporary markings which must be subsequently removed from the ultimate pavement, an approved removable lane tape will be used; however removable tape will be measured and paid as Pavement Striping-Temporary Paint 6"; and
3. Edge lines will be required for temporary striping; and
4. Existing, temporary, or permanent striping will be in place before a lane is opened to Traffic.
5. Place permanent striping on pavement within the project limits.
6. Permanent striping will be Permanent Paint.

Should the Contractor change the existing striping pattern, the Contractor is to restripe the roadway back to its original configuration within the time allotted for a lane closure.

PAVEMENT EDGE DROP-OFFS

Pavement edge drop-offs will be protected by a lane or shoulder closure. Lane closures will be protected with grabber delineator cones, vertical panels, or barricades as shown on the Standard Drawings.

A pavement edge between opposing directions of traffic or lanes that traffic is expected to cross in a lane

Traffic Control Plan
Fayette County and Scott County
I-75
Page 6 of 7

change situation shall not have an elevation difference greater than 1 ½". Place warning signs (MUTCD W8-11 or W8-9A) in advance of and at 1500' intervals throughout the drop-off area. Dual posting on both sides of the traveled way shall be required. Pavement edges that traffic is not expected to cross, except accidentally, shall be treated as follows:

Less than 2" – Protect with a lane closure.

2" to 4" – Protect with a lane closure. Place barrels, vertical panels, or barricades every 50 feet. Traffic cones may not be used in place of barrels, panels, and barricades at any time. Construct a wedge with compacted cuttings from milling, trenching, or asphalt mixtures with a 3:1 or flatter slope, when work is not active in the drop-off area. Place Type III Barricades at the beginning of the lane closures, and place additional Type III Barricades spaced at 2,500 feet during the time the lane closure is in place.

Guardrail Installation – Guardrail will be removed at the last practical moment and replaced as soon as the placement of asphalt in an area requiring guardrail is complete. All areas from which guardrail is removed shall be protected by a shoulder closure or other method approved by the Engineer until the new guardrail is installed.

TRAFFIC COORDINATOR

Designate an employee to be Traffic Coordinator. The designated Traffic Coordinator must be certified by an agency qualified for training in this area. The Traffic Coordinator will inspect the project maintenance of traffic once every two hours during the Contractor's operations and at any time a lane closure is in place. The Traffic Coordinator will report all incidents throughout the work zone to the Engineer on the project. The Contractor will furnish the name and telephone number where the Traffic Coordinator can be contacted always.

During any period when a lane closure is in place, the Traffic Coordinator will arrange for personnel to be present on the project always to inspect the traffic control, maintain the signing and devices, and relocate portable changeable message boards as queue lengths change. The personnel will have access on the project to a radio or telephone to be used in case of emergencies or accidents.

COORDINATION OF WORK

The Contractor is advised that other projects may be in progress within or in the near vicinity of this project. The traffic control of those projects may affect this project and the traffic control of this project may affect those projects. The Contractor will coordinate the work on this project with the work of the other contractors. In case of conflict, the Engineer will determine the relative priority to give to work phasing on the various projects.

CONTRACTOR'S AND CONTRACTOR'S EMPLOYEES' VEHICLES

Do not use or allow employees to use median crossovers at any time except when inside lanes are closed for construction. In all other phases of construction, change vehicular direction of travel only at interchanges.

Traffic Control Plan
Fayette County and Scott County
I-75
Page 7 of 7

LAW ENFORCEMENT OFFICERS (LEO'S)

Police support shall be a unit consisting of an off-duty police officer from any police force agency having lawful jurisdiction and a police car equipped with externally mounted flashing blue lights. Officers may be asked to issue citations for traffic violations but will be considered incidental to the contract unit bid price for "Law Enforcement Officer". No additional compensation will be provided. The officers will be placed at the discretion of the Engineer. Police support will be measured and paid on a per hour basis for each officer and police vehicle.

MATERIAL SUMMARY

CONTRACT ID: 181021

121GR18D021-NHPP

DE03400751821

I-75 MILL AND THIN ASPHALT OVERLAY ON I-75 FROM MP 111.95 TO MP 120.79 IN FAYETTE COUNTY.
 ASPHALT PAVEMENT & ROADWAY REHAB, A DISTANCE OF 8.8 MILES.

| Project Line No | Bid Code | DESCRIPTION | Quantity | Unit |
|-----------------|------------|--|--------------|-------|
| 0005 | 00001 | DGA BASE | 14.00 | TON |
| 0010 | 00193 | ASPHALT SCRATCH COURSE PG76-22 | 1,972.00 | TON |
| 0015 | 00342 | CL4 ASPH SURF 0.38A PG76-22 | 45,524.00 | TON |
| 0020 | 00356 | ASPHALT MATERIAL FOR TACK | 233.00 | TON |
| 0025 | 02676 | MOBILIZATION FOR MILL & TEXT - FAYETTE | 1.00 | LS |
| 0030 | 02677 | ASPHALT PAVE MILLING & TEXTURING | 41,840.00 | TON |
| 0035 | 02696 | SHOULDER RUMBLE STRIPS | 248,188.00 | LF |
| 0040 | 20071EC | JOINT ADHESIVE | 447,843.00 | LF |
| 0045 | 20757ED | PAVEMENT REPAIR | 785.00 | SQYD |
| 0050 | 23229EC | HIGH FRICTION SURFACE TREATMENT | 1,250.00 | SQYD |
| 0055 | 24781EC | INTELLIGENT COMPACTION FOR ASPHALT | 45,524.00 | TON |
| 0060 | 24891EC | PAVE MOUNT INFRARED TEMP EQUIPMENT | 4,966,237.00 | SF |
| 0065 | 02367 | GUARDRAIL END TREATMENT TYPE 1 | 1.00 | EACH |
| 0070 | 02381 | REMOVE GUARDRAIL | 175.00 | LF |
| 0075 | 02562 | TEMPORARY SIGNS | 2,000.00 | SQFT |
| 0080 | 02650 | MAINTAIN & CONTROL TRAFFIC - FAYETTE | 1.00 | LS |
| 0085 | 02671 | PORTABLE CHANGEABLE MESSAGE SIGN | 10.00 | EACH |
| 0090 | 02775 | ARROW PANEL | 4.00 | EACH |
| 0095 | 06401 | FLEXIBLE DELINEATOR POST-M/W | 533.00 | EACH |
| 0100 | 06404 | FLEXIBLE DELINEATOR POST-M/Y | 233.00 | EACH |
| 0105 | 06412 | STEEL POST MILE MARKERS | 14.00 | EACH |
| 0110 | 06511 | PAVE STRIPING-TEMP PAINT-6 IN | 581,167.00 | LF |
| 0115 | 06556 | PAVE STRIPING-DUR TY 1-6 IN W | 3,764.00 | LF |
| 0120 | 06557 | PAVE STRIPING-DUR TY 1-6 IN Y | 2,675.00 | LF |
| 0125 | 06568 | PAVE MARKING-THERMO STOP BAR-24IN | 90.00 | LF |
| 0130 | 06569 | PAVE MARKING-THERMO CROSS-HATCH | 10,719.00 | SQFT |
| 0135 | 06573 | PAVE MARKING-THERMO STR ARROW | 2.00 | EACH |
| 0140 | 06574 | PAVE MARKING-THERMO CURV ARROW | 43.00 | EACH |
| 0145 | 06575 | PAVE MARKING-THERMO COMB ARROW | 7.00 | EACH |
| 0150 | 10020NS | FUEL ADJUSTMENT | 73,931.00 | DOLL |
| 0155 | 10030NS | ASPHALT ADJUSTMENT | 185,694.00 | DOLL |
| 0160 | 20100ES842 | PAVE MARK TEMP PAINT LINE ARROW | 50.00 | EACH |
| 0165 | 20208NC | PAVE MARK-PAINT ARROWS | 9.00 | EACH |
| 0170 | 20411ED | LAW ENFORCEMENT OFFICER | 1,000.00 | HOURL |
| 0175 | 21802EN | G/R STEEL W BEAM-S FACE (7 FT POST) | 225.00 | LF |
| 0180 | 22692NS714 | PAVEMENT MARKING-THERMO LETTERS | 8.00 | EACH |
| 0185 | 24189ER | DURABLE WATERBORNE MARKING-6 IN W | 172,449.00 | LF |
| 0190 | 24190ER | DURABLE WATERBORNE MARKING-6 IN Y | 118,134.00 | LF |
| 0195 | 01020 | PERF PIPE HEADWALL TY 1-4 IN | 1.00 | EACH |
| 0200 | 24191ER | DURABLE WATERBORNE MARKING-12 IN W | 10,649.00 | LF |
| 0205 | 24489EC | INLAID PAVEMENT MARKER | 3,475.00 | EACH |
| 0210 | 24899EC | PAVE MARKING-THERMO ELONG ROUTE SHIELD | 10.00 | EACH |
| 0215 | 04793 | CONDUIT-1 1/4 IN | 425.00 | LF |

MATERIAL SUMMARY

| Project Line No | Bid Code | DESCRIPTION | Quantity | Unit |
|-----------------|------------|---|-----------|------|
| 0220 | 04795 | CONDUIT-2 IN | 155.00 | LF |
| 0225 | 04811 | ELECTRICAL JUNCTION BOX TYPE B | 5.00 | EACH |
| 0230 | 04820 | TRENCHING AND BACKFILLING | 530.00 | LF |
| 0235 | 04829 | PIEZOELECTRIC SENSOR | 50.00 | EACH |
| 0240 | 04830 | LOOP WIRE | 13,550.00 | LF |
| 0245 | 04871 | POLE 35 FT WOODEN | 1.00 | EACH |
| 0250 | 04895 | LOOP SAW SLOT AND FILL | 2,830.00 | LF |
| 0255 | 04899 | ELECTRICAL SERVICE | 1.00 | EACH |
| 0260 | 20359NN | GALVANIZED STEEL CABINET | 6.00 | EACH |
| 0265 | 20360ES818 | WOOD POST | 12.00 | EACH |
| 0270 | 20391NS835 | ELECTRICAL JUNCTION BOX TYPE A | 10.00 | EACH |
| 0275 | 21543EN | BORE AND JACK CONDUIT | 150.00 | LF |
| 0280 | 23206EC | INSTALL CONTROLLER CABINET | 1.00 | EACH |
| 0285 | 02568 | MOBILIZATION | 1.00 | LS |
| 0290 | 02569 | DEMOBILIZATION | 1.00 | LS |
| 0295 | 04792 | CONDUIT-1 IN - (ADDED: 6-19-18) | 110.00 | LF |
| 0300 | 04795 | CONDUIT-2 IN - (ADDED: 6-19-18) | 420.00 | LF |
| 0305 | 04811 | ELECTRICAL JUNCTION BOX TYPE B - (ADDED: 6-19-18) | 7.00 | EACH |
| 0310 | 04820 | TRENCHING AND BACKFILLING - (ADDED: 6-19-18) | 615.00 | LF |
| 0315 | 04830 | LOOP WIRE | 3,830.00 | LF |
| 0320 | 04850 | CABLE-NO. 14/1 PAIR - (ADDED: 6-19-18) | 4,295.00 | LF |
| 0325 | 04895 | LOOP SAW SLOT AND FILL - (ADDED: 6-19-18) | 1,545.00 | LF |
| 0330 | 24900EC | PVC CONDUIT-1 1/4 IN-SCHEDULE 80 - (ADDED: 6-19-18) | 560.00 | LF |
| 0335 | 24963ED | LOOP TEST - (ADDED: 6-19-18) | 21.00 | EACH |
| 0340 | 20509ED | BLOW UP/RELIEF JOINT - (ADDED: 6-19-18) | 75.00 | SQYD |
| 0345 | 20757ED | PAVEMENT REPAIR - (MAJOR) (ADDED: 6-19-18) | 200.00 | SQYD |

MATERIAL SUMMARY

CONTRACT ID: 181021

121GR18D021-NHPP

DE10500751821

I-75 MILL AND THIN ASPHALT OVERLAY ON I-75 FROM MP 120.79 TO MP 121.12 IN SCOTT COUNTY ASPHALT PAVEMENT & ROADWAY REHAB, A DISTANCE OF .3 MILES.

| Project Line No | Bid Code | DESCRIPTION | Quantity | Unit |
|-----------------|----------|--------------------------------------|------------|-------|
| 0350 | 00193 | ASPHALT SCRATCH COURSE PG76-22 | 239.00 | TON |
| 0355 | 00342 | CL4 ASPH SURF 0.38A PG76-22 | 1,467.00 | TON |
| 0360 | 00356 | ASPHALT MATERIAL FOR TACK | 7.00 | TON |
| 0365 | 02676 | MOBILIZATION FOR MILL & TEXT - SCOTT | 1.00 | LS |
| 0370 | 02677 | ASPHALT PAVE MILLING & TEXTURING | 1,706.00 | TON |
| 0375 | 02696 | SHOULDER RUMBLE STRIPS | 7,121.00 | LF |
| 0380 | 20071EC | JOINT ADHESIVE | 14,031.00 | LF |
| 0385 | 24781EC | INTELLIGENT COMPACTION FOR ASPHALT | 1,467.00 | TON |
| 0390 | 24891EC | PAVE MOUNT INFRARED TEMP EQUIPMENT | 156,664.00 | SF |
| 0395 | 02562 | TEMPORARY SIGNS | 500.00 | SQFT |
| 0400 | 02650 | MAINTAIN & CONTROL TRAFFIC - SCOTT | 1.00 | LS |
| 0405 | 02671 | PORTABLE CHANGEABLE MESSAGE SIGN | 2.00 | EACH |
| 0410 | 06401 | FLEXIBLE DELINEATOR POST-M/W | 9.00 | EACH |
| 0415 | 06412 | STEEL POST MILE MARKERS | 2.00 | EACH |
| 0420 | 06511 | PAVE STRIPING-TEMP PAINT-6 IN | 17,560.00 | LF |
| 0425 | 10020NS | FUEL ADJUSTMENT | 2,654.00 | DOLL |
| 0430 | 10030NS | ASPHALT ADJUSTMENT | 6,666.00 | DOLL |
| 0435 | 20411ED | LAW ENFORCEMENT OFFICER | 500.00 | HOURL |
| 0440 | 24189ER | DURABLE WATERBORNE MARKING-6 IN W | 5,281.00 | LF |
| 0445 | 24190ER | DURABLE WATERBORNE MARKING-6 IN Y | 3,499.00 | LF |
| 0450 | 24489EC | INLAID PAVEMENT MARKER | 88.00 | EACH |
| 0455 | 02568 | MOBILIZATION | 1.00 | LS |
| 0460 | 02569 | DEMOBILIZATION | 1.00 | LS |

PROPOSAL BID ITEMS

Report Date 6/19/18

Page 1 of 3

Section: 0001 - PAVING

| LINE | BID CODE | ALT | DESCRIPTION | QUANTITY | UNIT | UNIT PRIC | FP | AMOUNT |
|------|----------|-----|--|--------------|------|-----------|----|--------|
| 0010 | 00001 | | DGA BASE | 14.00 | TON | | \$ | |
| 0020 | 00193 | | ASPHALT SCRATCH COURSE PG76-22 | 2,211.00 | TON | | \$ | |
| 0030 | 00342 | | CL4 ASPH SURF 0.38A PG76-22 | 46,991.00 | TON | | \$ | |
| 0040 | 00356 | | ASPHALT MATERIAL FOR TACK | 240.00 | TON | | \$ | |
| 0050 | 02676 | | MOBILIZATION FOR MILL & TEXT FAYETTE | 1.00 | LS | | \$ | |
| 0060 | 02676 | | MOBILIZATION FOR MILL & TEXT SCOTT | 1.00 | LS | | \$ | |
| 0070 | 02677 | | ASPHALT PAVE MILLING & TEXTURING | 43,546.00 | TON | | \$ | |
| 0080 | 02696 | | SHOULDER RUMBLE STRIPS | 255,309.00 | LF | | \$ | |
| 0090 | 20071EC | | JOINT ADHESIVE | 461,874.00 | LF | | \$ | |
| 0095 | 20509ED | | BLOW UP/RELIEF JOINT (ADDED: 6-19-18) | 75.00 | SQYD | | \$ | |
| 0100 | 20757ED | | PAVEMENT REPAIR | 785.00 | SQYD | | \$ | |
| 0101 | 20757ED | | PAVEMENT REPAIR (MAJOR) (ADDED: 6-19-18) | 200.00 | SQYD | | \$ | |
| 0110 | 23229EC | | HIGH FRICTION SURFACE TREATMENT | 1,250.00 | SQYD | | \$ | |
| 0120 | 24781EC | | INTELLIGENT COMPACTION FOR ASPHALT | 46,991.00 | TON | | \$ | |
| 0130 | 24891EC | | PAVE MOUNT INFRARED TEMP EQUIPMENT | 5,122,901.00 | SF | | \$ | |

Section: 0002 - ROADWAY

| LINE | BID CODE | ALT | DESCRIPTION | QUANTITY | UNIT | UNIT PRIC | FP | AMOUNT |
|------|------------|-----|---------------------------------------|------------|------|-----------|----|--------------|
| 0140 | 01020 | | PERF PIPE HEADWALL TY 1-4 IN | 1.00 | EACH | | \$ | |
| 0150 | 02367 | | GUARDRAIL END TREATMENT TYPE 1 | 1.00 | EACH | | \$ | |
| 0160 | 02381 | | REMOVE GUARDRAIL | 175.00 | LF | | \$ | |
| 0170 | 02562 | | TEMPORARY SIGNS | 2,500.00 | SQFT | | \$ | |
| 0180 | 02650 | | MAINTAIN & CONTROL TRAFFIC FAYETTE | 1.00 | LS | | \$ | |
| 0190 | 02650 | | MAINTAIN & CONTROL TRAFFIC SCOTT | 1.00 | LS | | \$ | |
| 0200 | 02671 | | PORTABLE CHANGEABLE MESSAGE SIGN | 12.00 | EACH | | \$ | |
| 0210 | 02775 | | ARROW PANEL | 4.00 | EACH | | \$ | |
| 0220 | 06401 | | FLEXIBLE DELINEATOR POST-M/W | 542.00 | EACH | | \$ | |
| 0230 | 06404 | | FLEXIBLE DELINEATOR POST-M/Y | 233.00 | EACH | | \$ | |
| 0240 | 06412 | | STEEL POST MILE MARKERS | 16.00 | EACH | | \$ | |
| 0250 | 06511 | | PAVE STRIPING-TEMP PAINT-6 IN | 598,727.00 | LF | | \$ | |
| 0260 | 06556 | | PAVE STRIPING-DUR TY 1-6 IN W | 3,764.00 | LF | | \$ | |
| 0270 | 06557 | | PAVE STRIPING-DUR TY 1-6 IN Y | 2,675.00 | LF | | \$ | |
| 0280 | 06568 | | PAVE MARKING-THERMO STOP BAR-24IN | 90.00 | LF | | \$ | |
| 0290 | 06569 | | PAVE MARKING-THERMO CROSS-HATCH | 10,719.00 | SQFT | | \$ | |
| 0300 | 06573 | | PAVE MARKING-THERMO STR ARROW | 2.00 | EACH | | \$ | |
| 0310 | 06574 | | PAVE MARKING-THERMO CURV ARROW | 43.00 | EACH | | \$ | |
| 0320 | 06575 | | PAVE MARKING-THERMO COMB ARROW | 7.00 | EACH | | \$ | |
| 0330 | 10020NS | | FUEL ADJUSTMENT | 76,585.00 | DOLL | \$1.00 | \$ | \$76,585.00 |
| 0340 | 10030NS | | ASPHALT ADJUSTMENT | 192,360.00 | DOLL | \$1.00 | \$ | \$192,360.00 |
| 0350 | 20100ES842 | | PAVE MARK TEMP PAINT LINE ARROW | 50.00 | EACH | | \$ | |

PROPOSAL BID ITEMS

Report Date 6/19/18

| LINE | BID CODE | ALT | DESCRIPTION | QUANTITY | UNIT | UNIT PRIC | FP | AMOUNT |
|------|------------|-----|--|------------|-------|-----------|----|--------|
| 0360 | 20208NC | | PAVE MARK-PAINT ARROWS | 9.00 | EACH | | \$ | |
| 0370 | 20411ED | | LAW ENFORCEMENT OFFICER | 1,500.00 | HOURL | | \$ | |
| 0380 | 21802EN | | G/R STEEL W BEAM-S FACE (7 FT POST) | 225.00 | LF | | \$ | |
| 0390 | 22692NS714 | | PAVEMENT MARKING-THERMO LETTERS | 8.00 | EACH | | \$ | |
| 0400 | 24189ER | | DURABLE WATERBORNE MARKING-6 IN W | 177,730.00 | LF | | \$ | |
| 0410 | 24190ER | | DURABLE WATERBORNE MARKING-6 IN Y | 121,633.00 | LF | | \$ | |
| 0420 | 24191ER | | DURABLE WATERBORNE MARKING-12 IN W | 10,649.00 | LF | | \$ | |
| 0430 | 24489EC | | INLAID PAVEMENT MARKER | 3,563.00 | EACH | | \$ | |
| 0440 | 24899EC | | PAVE MARKING-THERMO ELONG ROUTE SHIELD | 10.00 | EACH | | \$ | |

Section: 0003 - TRAFFIC LOOPS

| LINE | BID CODE | ALT | DESCRIPTION | QUANTITY | UNIT | UNIT PRIC | FP | AMOUNT |
|------|------------|-----|--------------------------------|-----------|------|-----------|----|--------|
| 0450 | 04793 | | CONDUIT-1 1/4 IN | 425.00 | LF | | \$ | |
| 0460 | 04795 | | CONDUIT-2 IN | 155.00 | LF | | \$ | |
| 0470 | 04811 | | ELECTRICAL JUNCTION BOX TYPE B | 5.00 | EACH | | \$ | |
| 0480 | 04820 | | TRENCHING AND BACKFILLING | 530.00 | LF | | \$ | |
| 0490 | 04829 | | PIEZOELECTRIC SENSOR | 50.00 | EACH | | \$ | |
| 0500 | 04830 | | LOOP WIRE | 13,550.00 | LF | | \$ | |
| 0510 | 04871 | | POLE 35 FT WOODEN | 1.00 | EACH | | \$ | |
| 0520 | 04895 | | LOOP SAW SLOT AND FILL | 2,830.00 | LF | | \$ | |
| 0530 | 04899 | | ELECTRICAL SERVICE | 1.00 | EACH | | \$ | |
| 0540 | 20359NN | | GALVANIZED STEEL CABINET | 6.00 | EACH | | \$ | |
| 0550 | 20360ES818 | | WOOD POST | 12.00 | EACH | | \$ | |
| 0560 | 20391NS835 | | ELECTRICAL JUNCTION BOX TYPE A | 10.00 | EACH | | \$ | |
| 0570 | 21543EN | | BORE AND JACK CONDUIT | 150.00 | LF | | \$ | |
| 0580 | 23206EC | | INSTALL CONTROLLER CABINET | 1.00 | EACH | | \$ | |

Section: 0004 - SIGNAL LOOPS

| LINE | BID CODE | ALT | DESCRIPTION | QUANTITY | UNIT | UNIT PRIC | FP | AMOUNT |
|------|----------|-----|--|----------|------|-----------|----|--------|
| 0581 | 04792 | | CONDUIT-1 IN (ADDED: 6-19-18) | 110.00 | LF | | \$ | |
| 0582 | 04795 | | CONDUIT-2 IN (ADDED: 6-19-18) | 420.00 | LF | | \$ | |
| 0583 | 04811 | | ELECTRICAL JUNCTION BOX TYPE B (ADDED: 6-19-18) | 7.00 | EACH | | \$ | |
| 0584 | 04820 | | TRENCHING AND BACKFILLING (ADDED: 6-19-18) | 615.00 | LF | | \$ | |
| 0585 | 04830 | | LOOP WIRE | 3,830.00 | LF | | \$ | |
| 0586 | 04850 | | CABLE-NO. 14/1 PAIR (ADDED: 6-19-18) | 4,295.00 | LF | | \$ | |
| 0587 | 04895 | | LOOP SAW SLOT AND FILL (ADDED: 6-19-18) | 1,545.00 | LF | | \$ | |
| 0588 | 24900EC | | PVC CONDUIT-1 1/4 IN-SCHEDULE 80 (ADDED: 6-19-18) | 560.00 | LF | | \$ | |
| 0589 | 24963ED | | LOOP TEST (ADDED: 6-19-18) | 21.00 | EACH | | \$ | |

PROPOSAL BID ITEMS

Report Date 6/19/18

Section: 0005 - MOBILIZATION AND/OR DEMOBILIZATION

| LINE | BID CODE | ALT | DESCRIPTION | QUANTITY | UNIT | UNIT PRIC | FP | AMOUNT |
|------|----------|-----|----------------|----------|------|-----------|----|--------|
| 0590 | 02568 | | MOBILIZATION | 1.00 | LS | | \$ | |
| 0600 | 02569 | | DEMOBILIZATION | 1.00 | LS | | \$ | |